

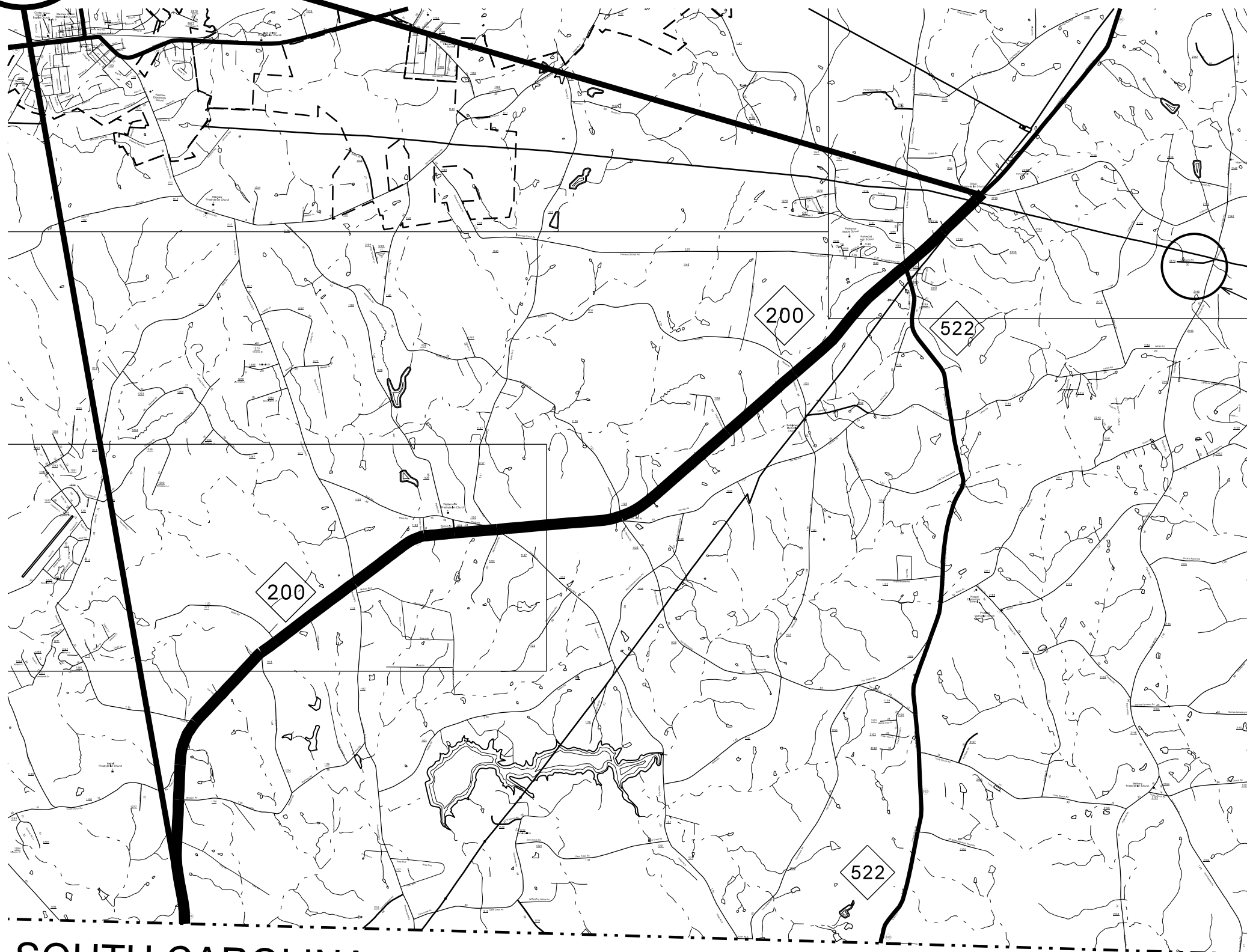
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STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2017CPT.10.18.10901.1 2017CPT.10.18.20901.1 - ETC.		
F.A. PROJECT NO.			



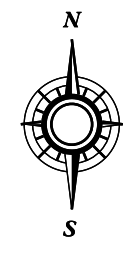
ENLARGED MUNICIPAL AND SUBURBAN AREAS  
**UNION COUNTY**  
NORTH CAROLINA

PREPARED BY THE  
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

**MAP #1**  
**NC 200 SOUTH**  
**(9.9 MILES)**  
**FROM PVMT JT. AT SR 2139 GRIFFITH ROAD**  
**TO SOUTH CAROLINA STATE LINE**

**SOUTH CAROLINA**

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2017CPT.10.18.10901.I 2017CPT.10.18.20901.I - ETC.	2	
F.A. PROJECT NO.			



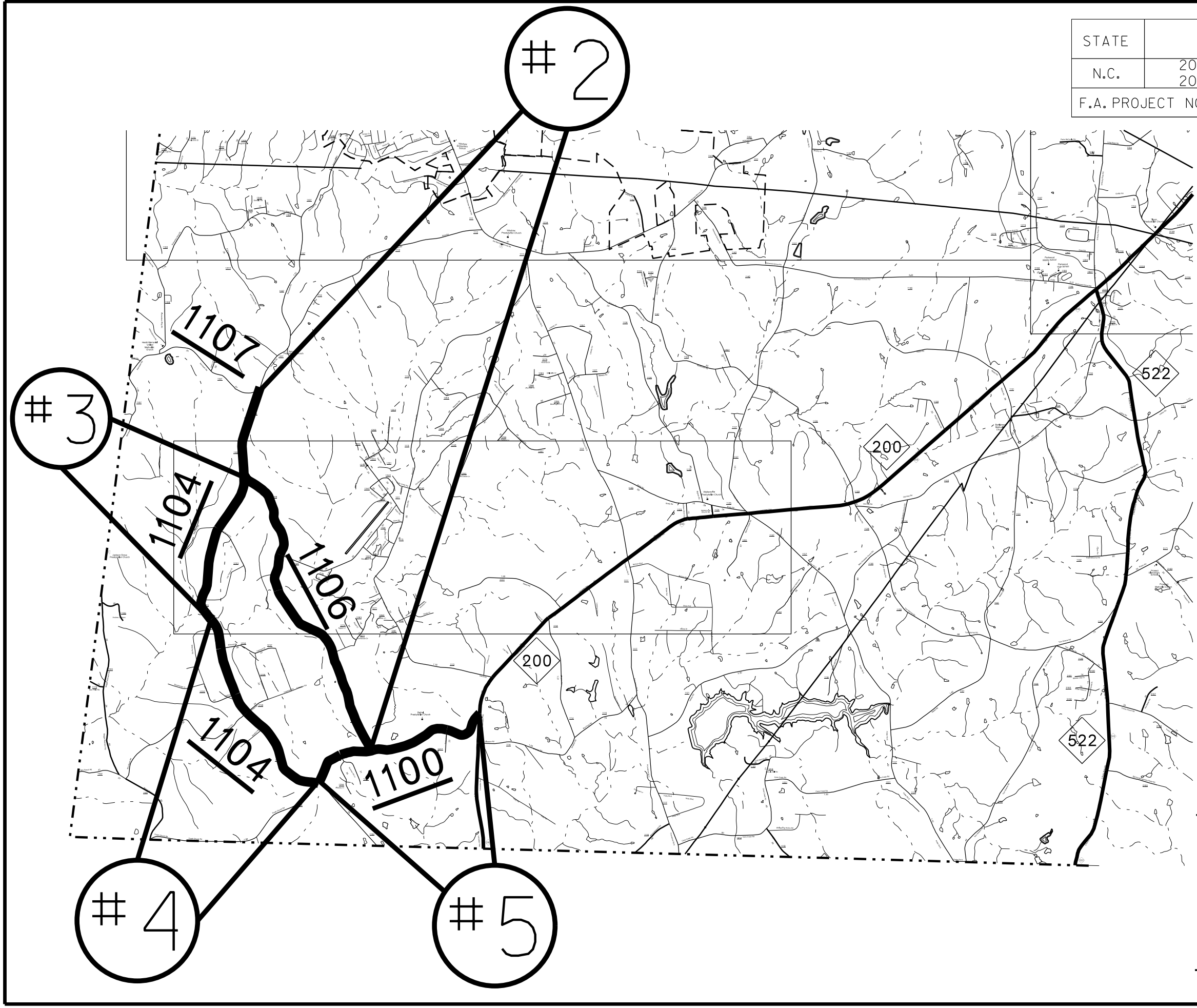
ENLARGED MUNICIPAL AND SUBURBAN AREAS  
**UNION COUNTY**  
 NORTH CAROLINA  
PREPARED BY THE  
 NORTH CAROLINA DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

**MAP #2**  
 SR 1106 WALKUP ROAD  
 (4.02 MILES)  
 FROM SR 1107 REOHBETH ROAD  
 TO SR 1100 TIRZAH CHURCH ROAD

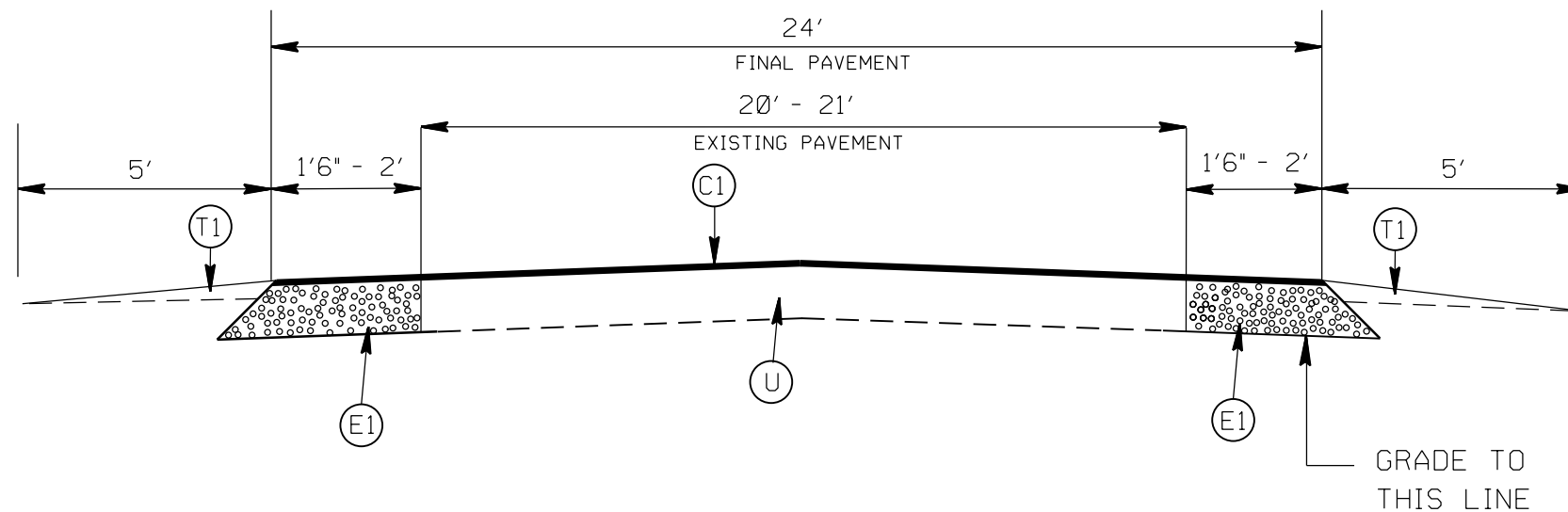
**MAP #3**  
 SR 1104 WAXHAW CREEK ROAD  
 (1.54 MILES)  
 FROM SR 1106 WALKUP ROAD  
 TO PVMT JT. AT BRIDGE

**MAP #4**  
 SR 1104 WAXHAW CREEK ROAD  
 (0.51 MILES)  
 FROM PVMT JT. AT BRIDGE  
 TO SR 1100 TIRZAH CHURCH ROAD

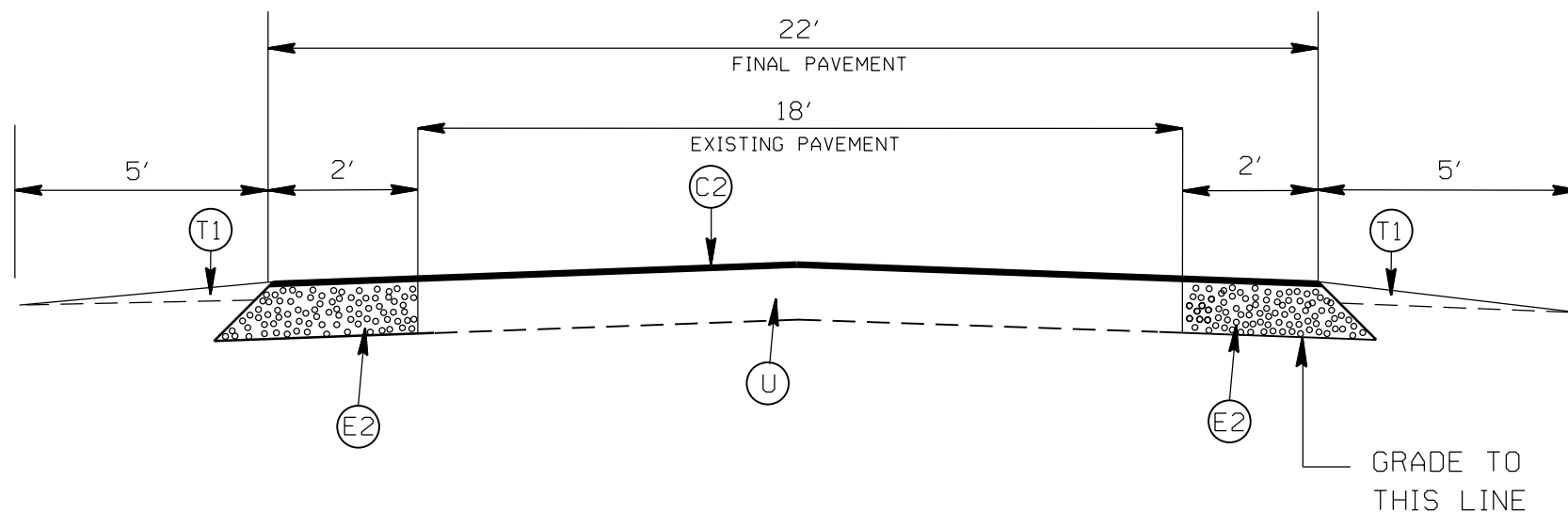
**MAP #5**  
 SR 1100 TIRZAH CHURCH ROAD  
 (1.88 MILES)  
 FROM NC 200 SOUTH  
 TO SR 1104 WAXHAW CREEK ROAD



STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2017CPT.10.18.10901.1 2017CPT.10.18.20901.1 - ETC.	3	
F.A. PROJECT NO.			



TYPICAL SECTION NO. 1  
NC 200 SOUTH (MAP 1)



TYPICAL SECTION NO. 2  
SR 1106 WALKUP ROAD (MAP 2)  
FROM STA 10+00 TO STA: 54+00  
\*SEE NOTE 2

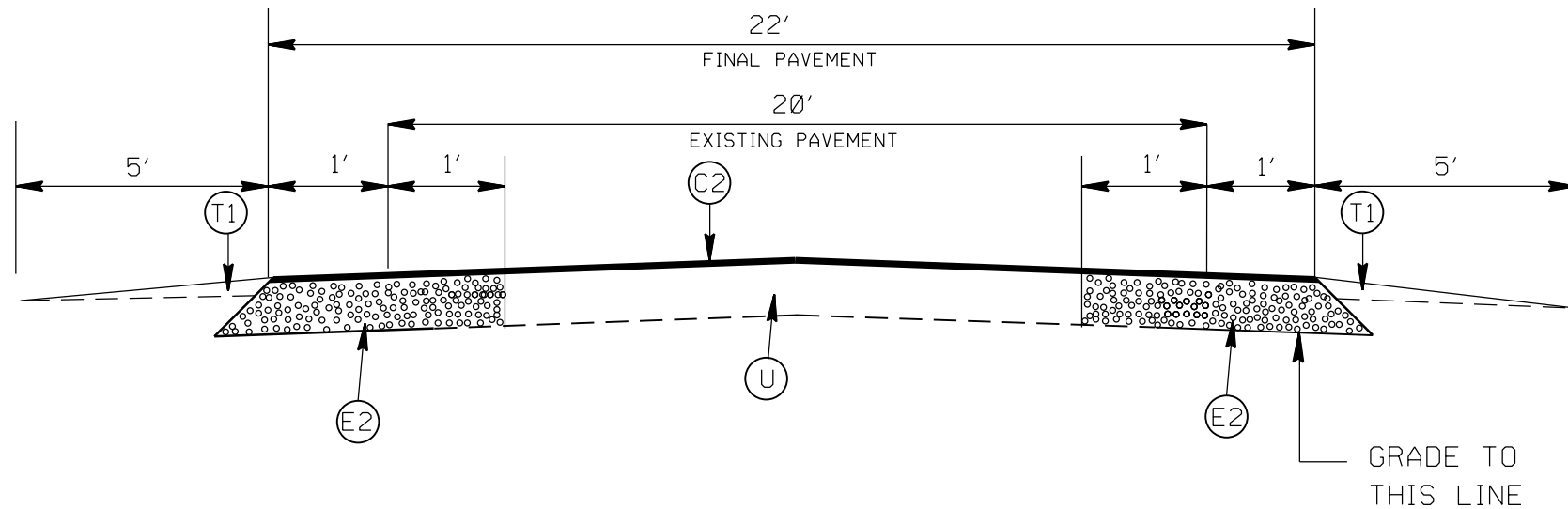
PAVEMENT SCHEDULE

(C1)	PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
(C2)	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
(E1)	PROPOSED 8" OF ASPHALT CONC. BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE 456 LBS. PER SQ. YD. IN EACH OF TWO LIFTS.
(E2)	PROPOSED 5" OF ASPHALT CONC. BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE 570 LBS. PER SQ. YD.
(T1)	SHOULDER RECONSTRUCTION
(U)	EXISTING PAVEMENT

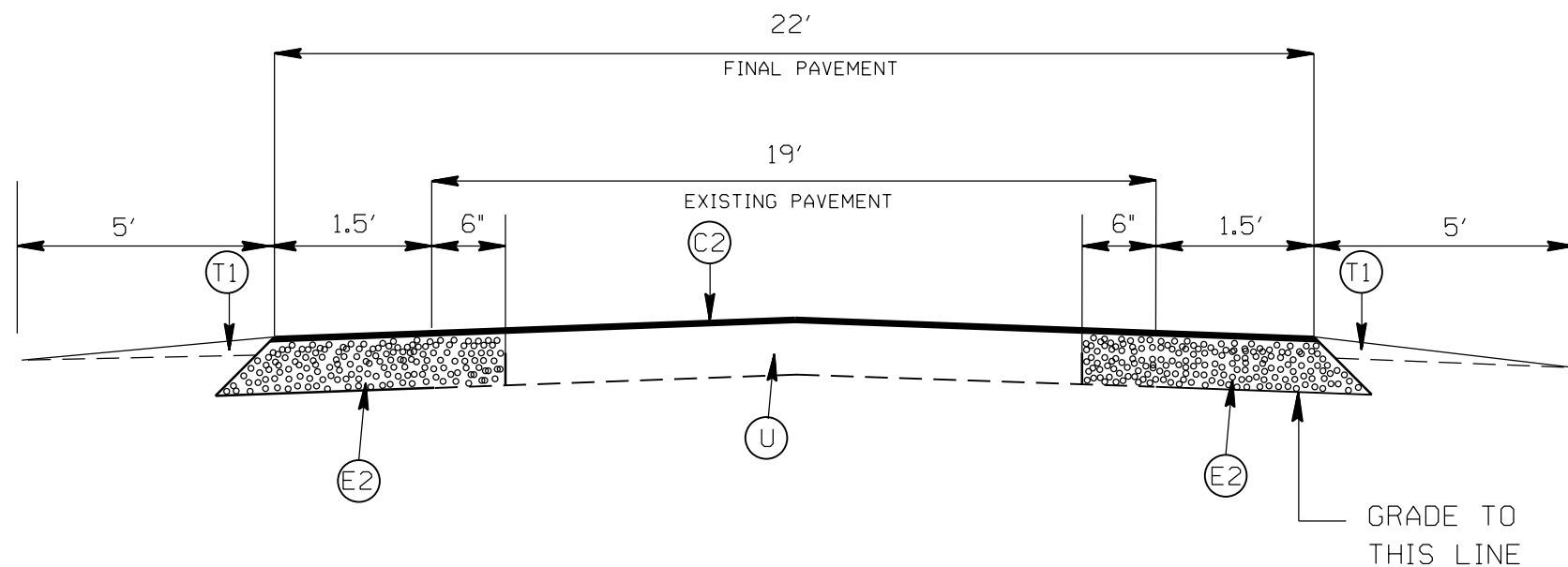
2017-2018  
UNION COUNTY RESURFACING

SCALE	-NA-		REVISIONS
DATE	04/17		
DWG. BY	AMO		
DESIGN BY	AMO		
APPROVED	CLA		

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2017CPT.10.18.10901.1 2017CPT.10.18.20901.1 - ETC.	4	
F.A. PROJECT NO.			



TYPICAL SECTION NO. 3  
 SR 1106 WALKUP ROAD (MAP 2)  
 FROM STA: STA: 54+00 TO END OF MAP  
 \*SEE NOTE 2  
 SR 1104 WAXHAW CREEK ROAD (MAP 3)



TYPICAL SECTION NO. 4  
 SR 1104 WAXHAW CREEK ROAD (MAP 4)  
 \*SEE NOTE 3  
 SR 1100 TIRZAH CHURCH ROAD (MAP 5)

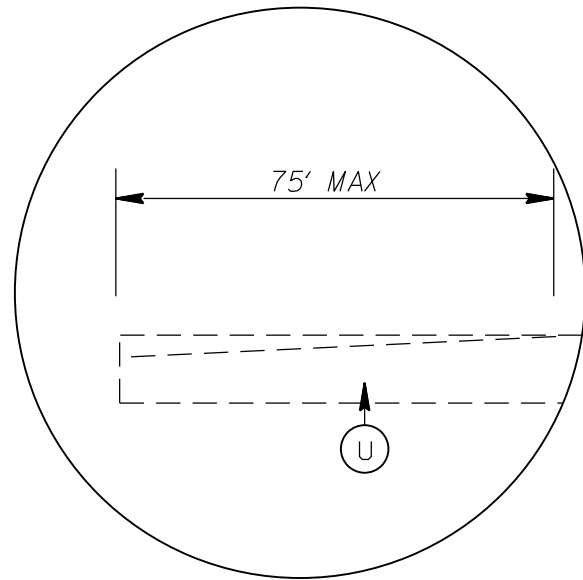
PAVEMENT SCHEDULE

(C1)	PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
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(E2)	PROPOSED 5" OF ASPHALT CONC. BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE 570 LBS. PER SQ. YD.
(T1)	SHOULDER RECONSTRUCTION
(U)	EXISTING PAVEMENT

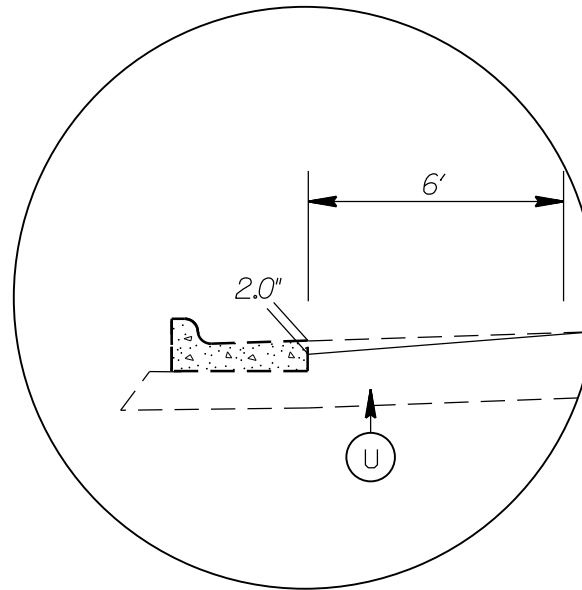
2017-2018  
 UNION COUNTY RESURFACING

SCALE	-NA-		REVISIONS
DATE	04/17		
DWG. BY	AMO		
DESIGN BY	AMO		
APPROVED	CLA		

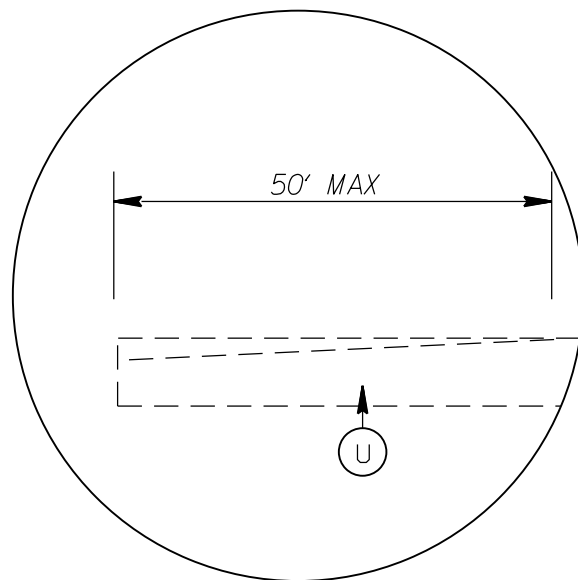
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2017CPT.10.18.10901.1 2017CPT.10.18.20901.1 - ETC.	5	
F.A. PROJECT NO.			



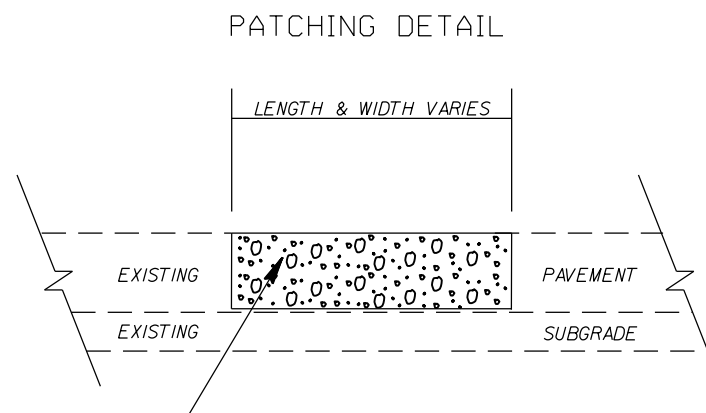
DETAIL FOR INCIDENTAL MILLING (0" TO 2.0")



DETAIL FOR PROFILE MILLING (0" TO 2.0")  
MAP 1



DETAIL FOR INCIDENTAL MILLING (0" TO 1.5")



RATE IS VARIABLE AND SHALL BE AS DIRECTED BY THE ENGINEER.  
ASPHALT TYPE 119.0C SHALL BE PLACED.

PAVEMENT SCHEDULE

(C1)	PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
(C2)	PROP. APPROX. 1.5" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
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(T1)	SHOULDER RECONSTRUCTION
(U)	EXISTING PAVEMENT

2017-2018  
UNION COUNTY RESURFACING

SCALE	-NA-		REVISIONS	
DATE	04/17			
DWG. BY	AMO			
DESIGN BY	AMO			
APPROVED	CLA			

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2017CPT.10.18.10901.I 2017CPT.10.18.20901.I - ETC.	6	
F.A. PROJECT NO.			

*NOTES:*

*1: LEVELING COURSE TO BE PLACED AT LOCATIONS AS DIRECTED BY THE ENGINEER.*

*2: ON MAP 2 MILL AND FILL BRIDGE 1.5",*

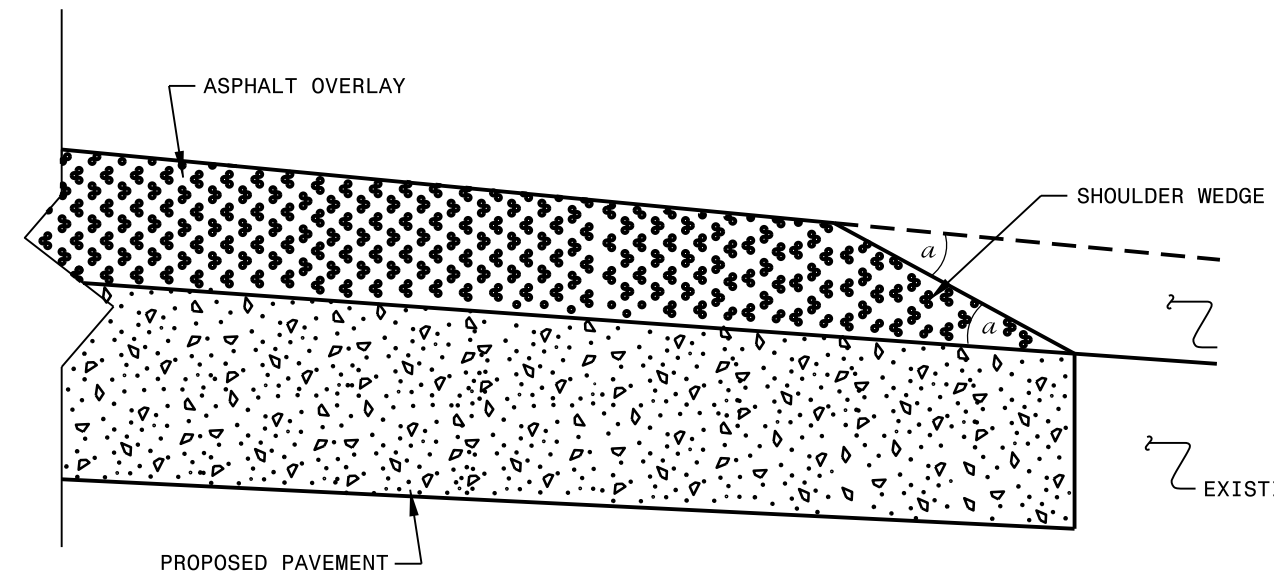
*3: ON MAP 4, SKIP CONCRETE BRIDGE.*

*4: SHOULDER RECONSTRUCTION WILL BE AS DIRECTED BY THE ENGINEER.*

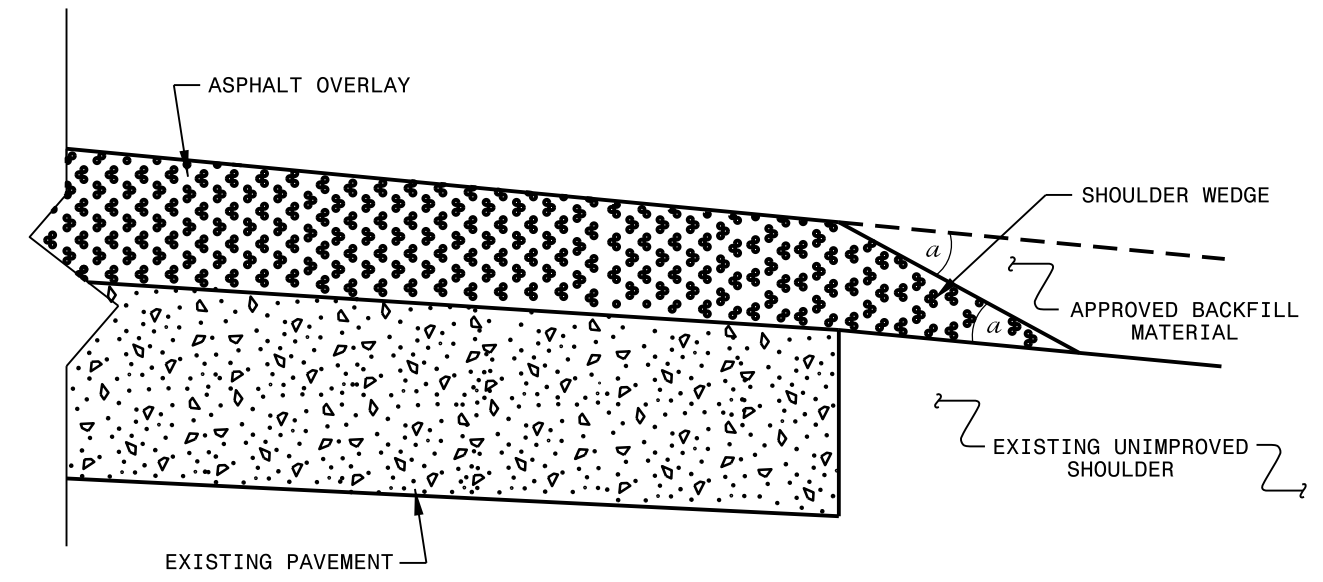
2017-2018  
UNION COUNTY RESURFACING

SCALE	-NA-		REVISIONS	
DATE	04/17			
DWG. BY	AMO			
DESIGN BY	AMO			
APPROVED	CLA			

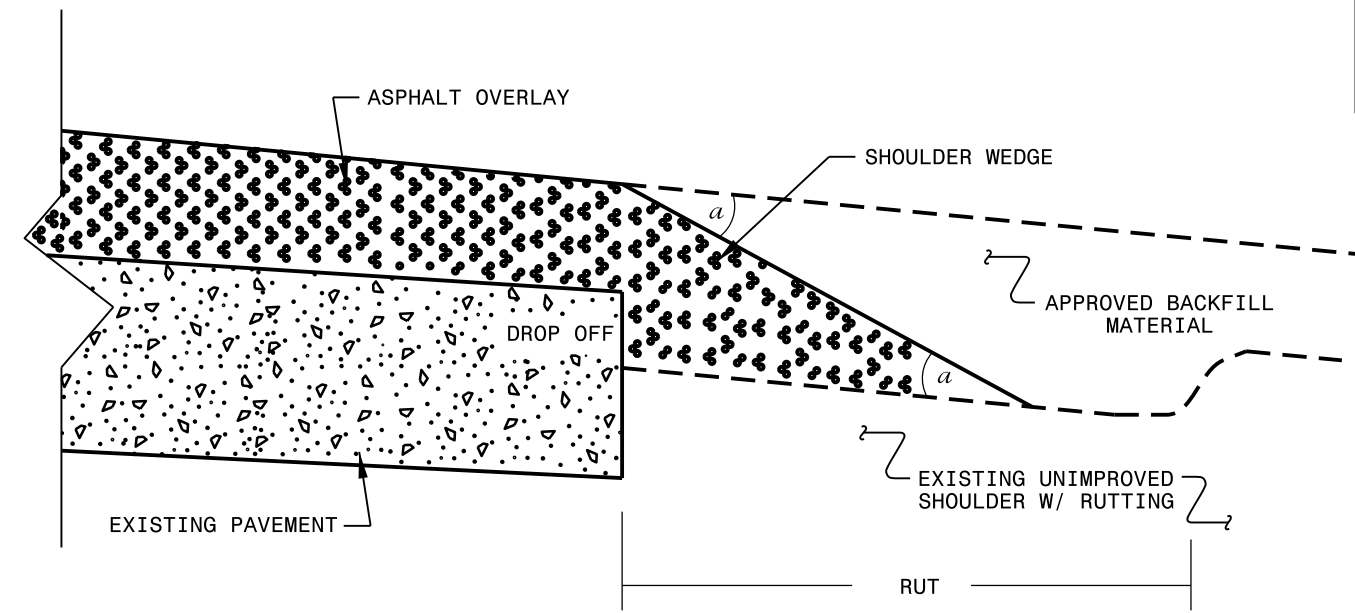
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
  - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
  - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ Widening or  
 with Existing Paved Shoulder having no dropoffs)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ NO Widening)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Adjacent to  
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS  
 AND DEVELOPMENT UNIT  
 Office 919-707-6950 FAX 919-250-4119

**SHOULDER WEDGE  
 DETAILS**

ORIGINAL BY: T.SPELL DATE: 7-19-11  
 MODIFIED BY: DATE: 2/2/16  
 CHECKED BY: DATE:  
 FILE SPEC.: susr/details/stand/shoulderwedgedetail.dgn

SYSTEMS DESIGN  
 USER NAME



PROJECT NO.	SHEET NO.	TOTAL NO.
2017CPT.10.18.10901.1, 2017CPT.10.18.20901.1-ETC.	8	

### SUMMARY OF QUANTITIES

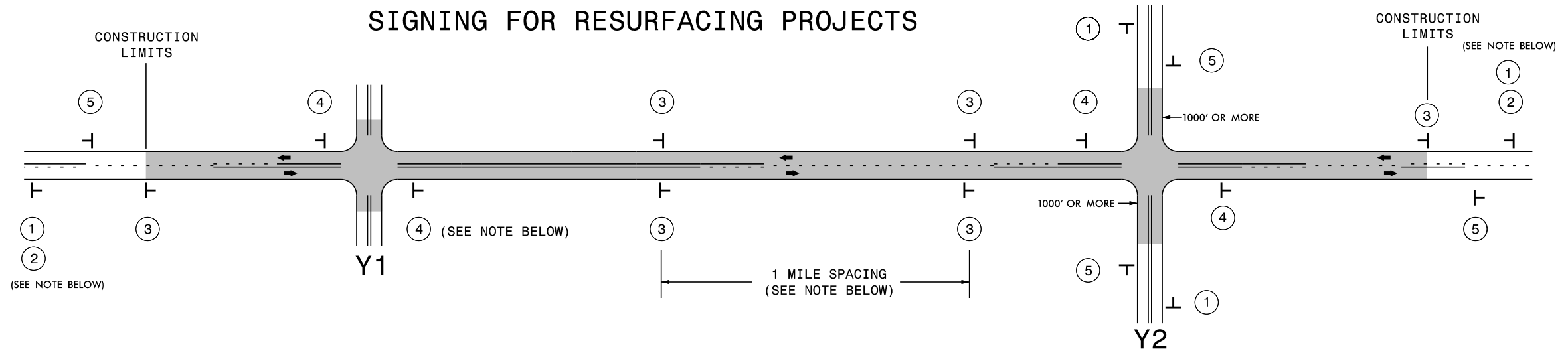
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	BORROW CY	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	1 1/2" MILLING SY	0" TO 2" MILLING SY	INCIDENTAL MILLING SY	BASE COURSE, B25.0C TONS	SURFACE COURSE, S9.5B TONS	LEVELING COURSE, S9.5B TONS	ASPHALT BINDER FOR PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	6" DRIVEWAYS SY	TEMPORARY SILT FENCE LF	STONE FOR EROSION CONTROL, CLASS B TN	SEDIMENT CONTROL STONE TN	WATTLE LF	POLYACRYLAMIDE (PAM) LB
2017CPT.10.18.10901.1	Union	1	NC 200 SOUTH/ROUTE 3000020089	FROM PAVEMENT JOINT AT SR 2139 GRIFFITH ROAD TO SOUTH CAROLINA STATE LINE MILEPOST 0.00 TO 9.9	1	2	2WU	NO	NO	9.9	24	1,700	1,240	20		60	560	11,699	17,690		1,576		400	2,970	300	149	2,970	4
<b>TOTAL FOR PROJ NO. 2017CPT.10.18.10901.1</b>										<b>9.9</b>		<b>1,700</b>	<b>1,240</b>	<b>20</b>		<b>60</b>	<b>560</b>	<b>11,699</b>	<b>17,690</b>		<b>1,576</b>		<b>400</b>	<b>2,970</b>	<b>300</b>	<b>149</b>	<b>2,970</b>	<b>4</b>
2017CPT.10.18.20901.1	Union	2	SR 1106 WALKUP ROAD/ROUTE 40001106894	FROM SR 1107 REOBETH ROAD TO SR 1100 TIRZAH CHURCH ROAD MILEPOST 4.02 TO 0.00	2,3	2	2WU	NO	NO	4.02	22	660	300	8	347		269	2,979	4,895	1,770	540	2,211	125	1,200	120	60	1,200	3
<b>TOTAL FOR PROJ NO. 2017CPT.10.18.20901.1</b>										<b>4.02</b>		<b>660</b>	<b>300</b>	<b>8</b>	<b>347</b>		<b>269</b>	<b>2,979</b>	<b>4,895</b>	<b>1,770</b>	<b>540</b>	<b>2,211</b>	<b>125</b>	<b>1,200</b>	<b>120</b>	<b>60</b>	<b>1,200</b>	<b>3</b>
2017CPT.10.18.20901.2	Union	3	SR 1104 WAXHAW CREEK ROAD/ROUTE 4000110489	FROM SR 1106 WALKUP ROAD TO PAVEMENT JOINT A BRIDGE MILEPOST 4.3 TO 2.87	3	2	2WU	NO	NO	1.54	22	250	115	3.1			245	1,130	1,832	678	204	763	50	462	46	23	462	1
<b>TOTAL FOR PROJ NO. 2017CPT.10.18.20901.2</b>										<b>1.54</b>		<b>250</b>	<b>115</b>	<b>3.1</b>			<b>245</b>	<b>1,130</b>	<b>1,832</b>	<b>678</b>	<b>204</b>	<b>763</b>	<b>50</b>	<b>462</b>	<b>46</b>	<b>23</b>	<b>462</b>	<b>1</b>
2017CPT.10.18.20901.3	Union	4	SR 1104 WAXHAW CREEK ROAD/ROUTE 4000110489	FROM PAVEMENT JOINT AT BRIDGE TO SR 1100 TIRZAH CHURCH ROAD MILEPOST 2.86 TO 0.84	4	2	2WU	NO	NO	0.51	22	83	21	1			285	374	720	225	74	252	25	153	16	8	153	1
<b>TOTAL FOR PROJ NO. 2017CPT.10.18.20901.3</b>										<b>0.51</b>		<b>83</b>	<b>21</b>	<b>1</b>			<b>285</b>	<b>374</b>	<b>720</b>	<b>225</b>	<b>74</b>	<b>252</b>	<b>25</b>	<b>153</b>	<b>16</b>	<b>8</b>	<b>153</b>	<b>1</b>
2017CPT.10.18.20901.4	Union	5	SR 1100 TIRZAH CHURCH ROAD/ROUTE 4000110089	FROM NC 200 SOUTH TO SR 1104 WAXHAW CREEK ROAD MILEPOST 1.88 TO 3.77	4	2	2WU	NO	NO	1.88	22	311	141	4			269	1,053	2,341	828	241	931	125	564	56	28	564	2
<b>TOTAL FOR PROJ NO. 2017CPT.10.18.20901.4</b>										<b>1.88</b>		<b>311</b>	<b>141</b>	<b>4</b>			<b>269</b>	<b>1,053</b>	<b>2,341</b>	<b>828</b>	<b>241</b>	<b>931</b>	<b>125</b>	<b>564</b>	<b>56</b>	<b>28</b>	<b>564</b>	<b>2</b>
<b>GRAND TOTAL</b>										<b>17.85</b>		<b>3,004</b>	<b>1,817</b>	<b>36.1</b>	<b>347</b>	<b>60</b>	<b>1,628</b>	<b>17,235</b>	<b>27,478</b>	<b>3,501</b>	<b>2,635</b>	<b>4,157</b>	<b>725</b>	<b>5,349</b>	<b>538</b>	<b>268</b>	<b>5,349</b>	<b>11</b>

PROJECT NO.	SHEET NO.	TOTAL NO.
2017CPT.10.18.10901.1	9	
2017CPT.10.18.20901.1, ETC.		

## THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	LENGTH	WIDTH	4413000000-E	4457000000-N	4510000000-N	4710000000-E	4721000000-E	4725000000-E	4810000000-E		4890000000-E		4900000000-N	
										WORK ZONE ADVANCE/GENERAL WARNING SIGNING SF	TEMPORARY TRAFFIC CONTROL LS	LAW ENFORCEMENT HR	24" X 120 M WHITE THERMO LF	THERMO MSG STOP 120 M EA	THERMO RT ARROW 90 M EA	4" YELLOW PAINT LF	4" WHITE PAINT LF	THERMOPLASTIC PAVEMENT MARKING LINES 4",90 MILS (HRM) LF	THERMOPLASTIC PAVEMENT MARKING LINES 4",120 MILS (HRM) LF	YELLOW & YELLOW MARKERS EA	CRYSTAL & RED MARKERS EA
2017CPT.10.18.10901.1	Union	1	NC 200 SOUTH/ROUTE 3000020089	FROM PAVEMENT JOINT AT SR 2139 GRIFFITH ROAD TO SOUTH CAROLINA STATE LINE MILEPOST 0.00 TO 9.9	1	2	2WU	9.9	24	358.0	1.00	40.00	200	8	1			106,000	81,241	656	4
<b>TOTAL FOR PROJ NO. 2017CPT.10.18.10901.1</b>								<b>9.9</b>		<b>358.0</b>	<b>1.00</b>	<b>40</b>	<b>200</b>	<b>8</b>	<b>1</b>			<b>106,000</b>	<b>81,241</b>	<b>656</b>	<b>4</b>
																					<b>660</b>
2017CPT.10.18.20901.1	Union	2	SR 1106 WALKUP ROAD/ROUTE 40001106894	FROM SR 1107 REOHBETH ROAD TO SR 1100 TIRZAH CHURCH ROAD MILEPOST 4.02 TO 0.00	2,3	2	2WU	4.02	22	358.0	*					84,170	85,820				
<b>TOTAL FOR PROJ NO. 2017CPT.10.18.20901.1</b>								<b>4.02</b>		<b>358.0</b>						<b>84,170</b>	<b>85,820</b>				
																	<b>169,990</b>				
2017CPT.10.18.20901.2	Union	3	SR 1104 WAXHAW CREEK ROAD/ROUTE 4000110489	FROM SR 1106 WALKUP ROAD TO PAVEMENT JOINT A BRIDGE MILEPOST 4.3 TO 2.87	3	2	2WU	1.54	22	358.0	*					30,302	32,432				
<b>TOTAL FOR PROJ NO. 2017CPT.10.18.20901.2</b>								<b>1.54</b>		<b>358.0</b>						<b>30,302</b>	<b>32,432</b>				
																	<b>62,734</b>				
2017CPT.10.18.20901.3	Union	4	SR 1104 WAXHAW CREEK ROAD/ROUTE 4000110489	FROM PAVEMENT JOINT AT BRIDGE TO SR 1100 TIRZAH CHURCH ROAD MILEPOST 2.86 TO 0.84	4	2	2WU	0.51	22	358.0	*					8,415	11,224				
<b>TOTAL FOR PROJ NO. 2017CPT.10.18.20901.3</b>								<b>0.51</b>		<b>358.0</b>						<b>8,415</b>	<b>11,224</b>				
																	<b>19,639</b>				
2017CPT.10.18.20901.4	Union	5	SR 1100 TIRZAH CHURCH ROAD/ROUTE 4000110089	FROM NC 200 SOUTH TO SR 1104 WAXHAW CREEK ROAD MILEPOST 1.88 TO 3.77	4	2	2WU	1.88	22	358.0	*					40,588	40,588				
<b>TOTAL FOR PROJ NO. 2017CPT.10.18.20901.4</b>								<b>1.88</b>		<b>358</b>						<b>40,588</b>	<b>40,588</b>				
																	<b>81,176</b>				
<b>GRAND TOTAL</b>								<b>17.85</b>		<b>1,790</b>	<b>1</b>	<b>40</b>	<b>200</b>	<b>8</b>	<b>1</b>	<b>163,475</b>	<b>170,064</b>	<b>106,000</b>	<b>81,241</b>	<b>656</b>	<b>4</b>
																	<b>333,539</b>				<b>660</b>

# SIGNING FOR RESURFACING PROJECTS



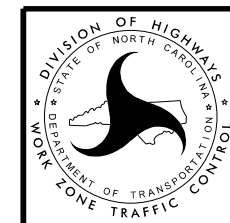
LEGEND	
⊥	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

## MAINLINE (-L-) SIGNING

## -Y- LINE SIGNING

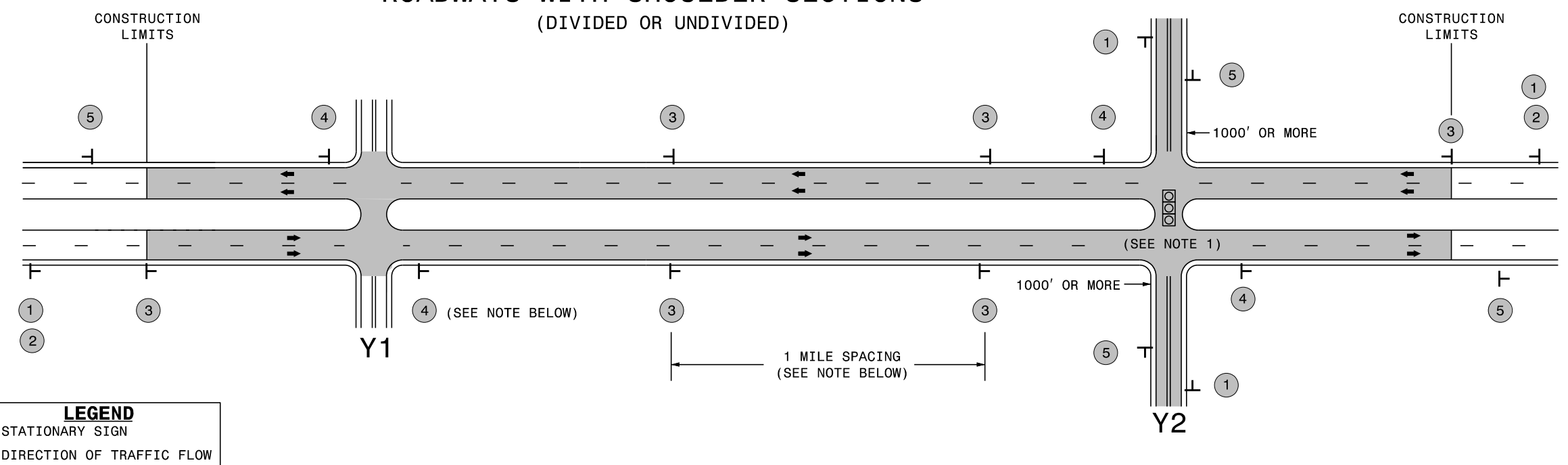
SIGNING NOTES AND PLACEMENT PER DIRECTION	MAINLINE (-L-) SIGNING		-Y- LINE SIGNING	
	①	 W20-1 48" X 48"	<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p>	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> <li>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>2) SUBDIVISION ROADS</li> <li>3) DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> <p>W20-1 48" X 48"</p> </div> <div style="text-align: center;"> <p>W20-7 A 48" X 48"</p> </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
	②	 W7-3aP 24" X 18"	<p>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	
	③	 SP 13107 48" X 48"	<p>PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.</p>	
	④	 SP 13106 48" X 48"	<p>THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p>	
⑤	 G20-2 A 48" X 24"	<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.</p>		

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RESURFACING  
 ADVANCE WARNING SIGNS  
 FOR  
 RURAL AND SUBURBAN  
 2 LANE ROADWAYS

## SIGNING FOR RURAL AND SUBURBAN MULTI-LANE ROADWAYS WITH SHOULDER SECTIONS (DIVIDED OR UNDIVIDED)



### MAINLINE (-L-) SIGNING

### -Y- LINE SIGNING

<b>SIGNING NOTES AND PLACEMENT PER DIRECTION</b>	1	 <small>W20-1 48" X 48"</small>	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.		
	2	 <small>W7-3aP 24" X 18"</small>	#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)		
	3	 <small>SP 13107 48" X 48"</small>	PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.		
	4	 <small>SP 13106 48" X 48"</small>	THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.		
5	 <small>G20-2 A 48" X 24"</small>	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.	<p style="text-align: center;"><b>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</b></p> <ol style="list-style-type: none"> <li>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>2) SUBDIVISION ROADS</li> <li>3) DEAD END ROADS</li> </ol> <p style="text-align: center;">WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around; align-items: center;"> <div style="text-align: center;">   <small>W20-1 48" X 48"</small> </div> <div style="text-align: center;">   <small>W20-7 A 48" X 48"</small> </div> </div> <p style="text-align: center;">PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p> <p><b>NOTES:</b></p> <ol style="list-style-type: none"> <li>1) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.</li> </ol>		

DIVISION OF HIGHWAYS  
STATE OF NORTH CAROLINA  
DEPARTMENT OF TRANSPORTATION  
WORK ZONE TRAFFIC CONTROL

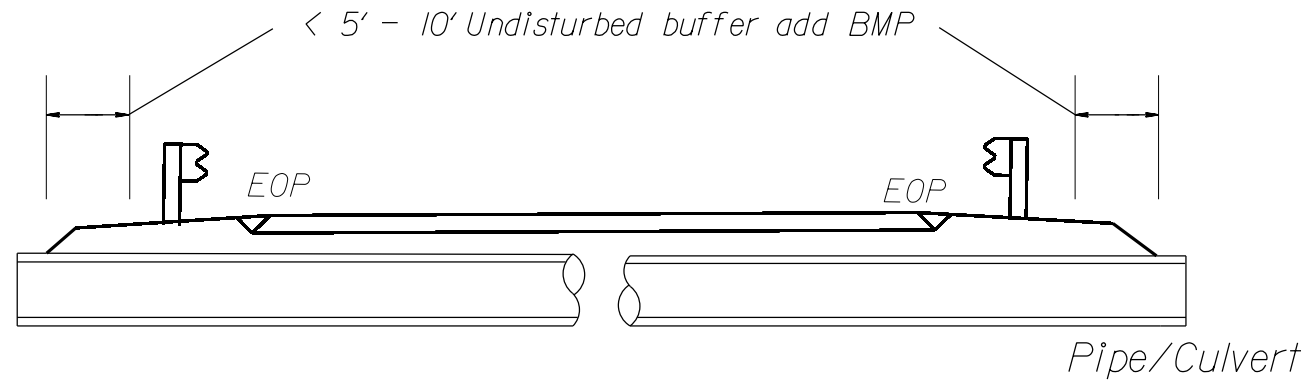
**RESURFACING  
ADVANCE WARNING SIGNS  
FOR RURAL AND SUBURBAN  
MULTI-LANE ROADWAYS  
W/ SHOULDER SECTIONS  
(DIVIDED OR UNDIVIDED)**

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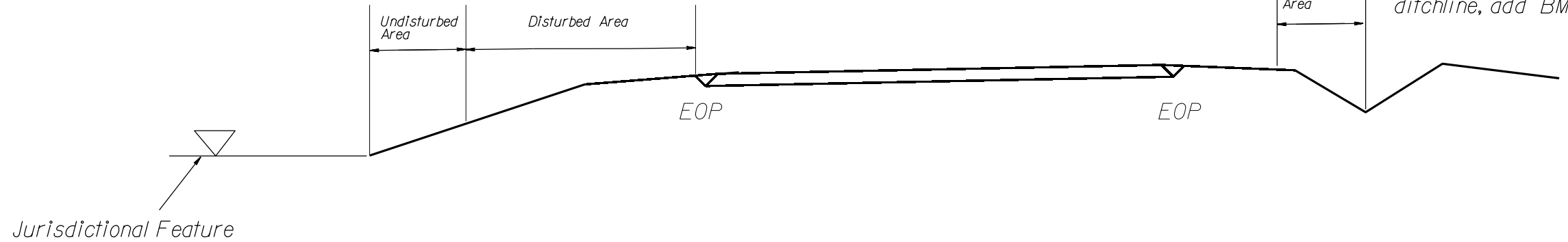
NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle or Silt Fence

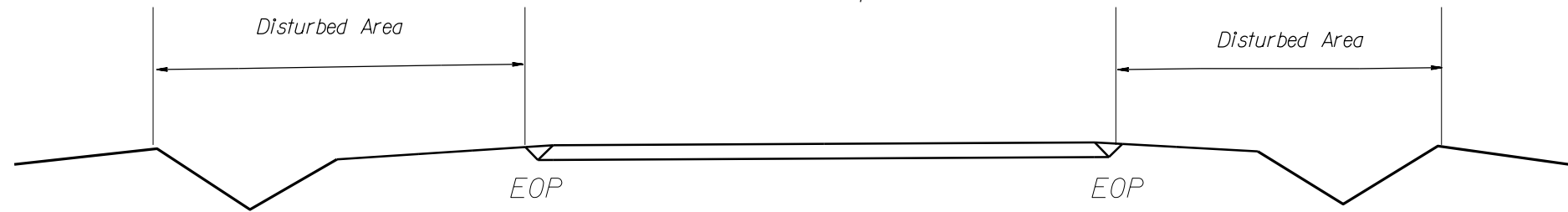
# EROSION CONTROL DETAIL



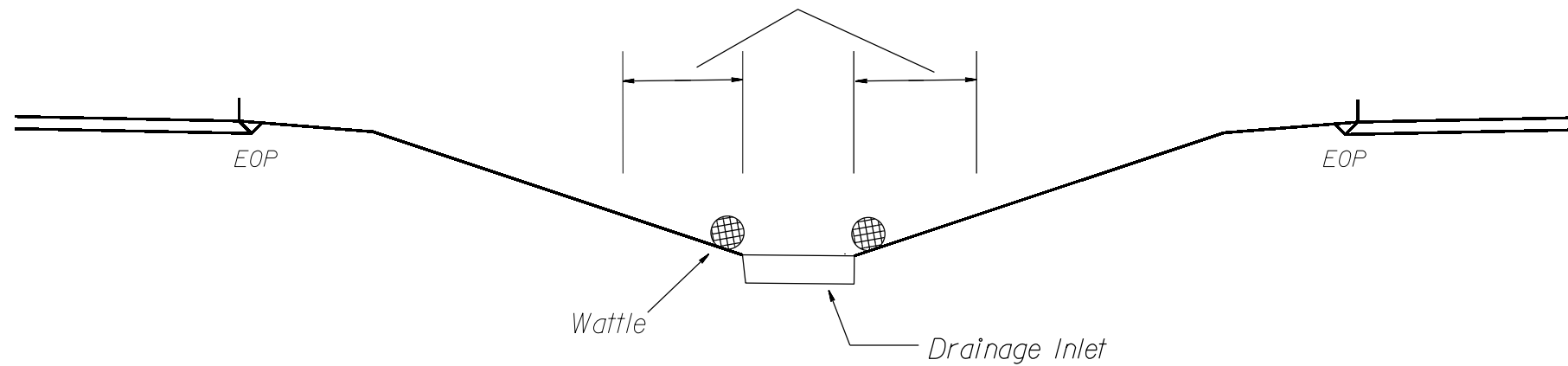
< 5' - 10' Undisturbed buffer from jurisdictional feature add BMP



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

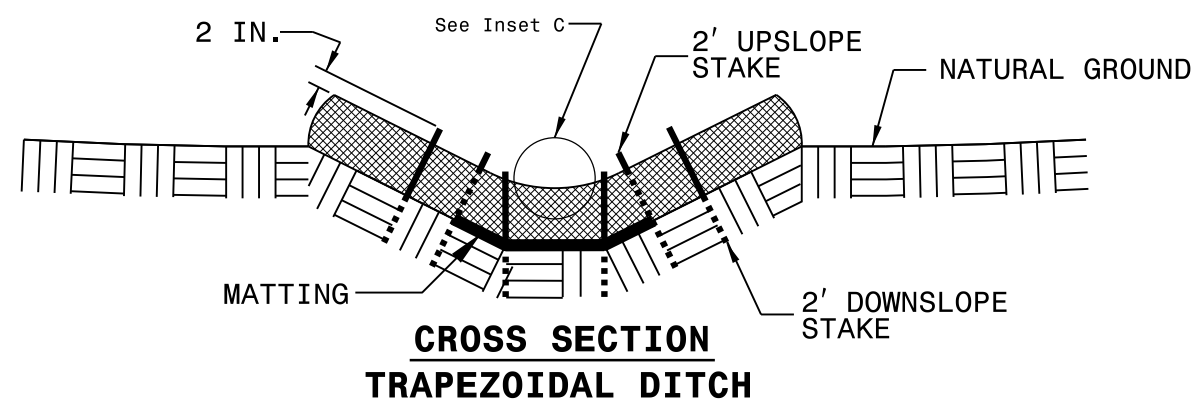
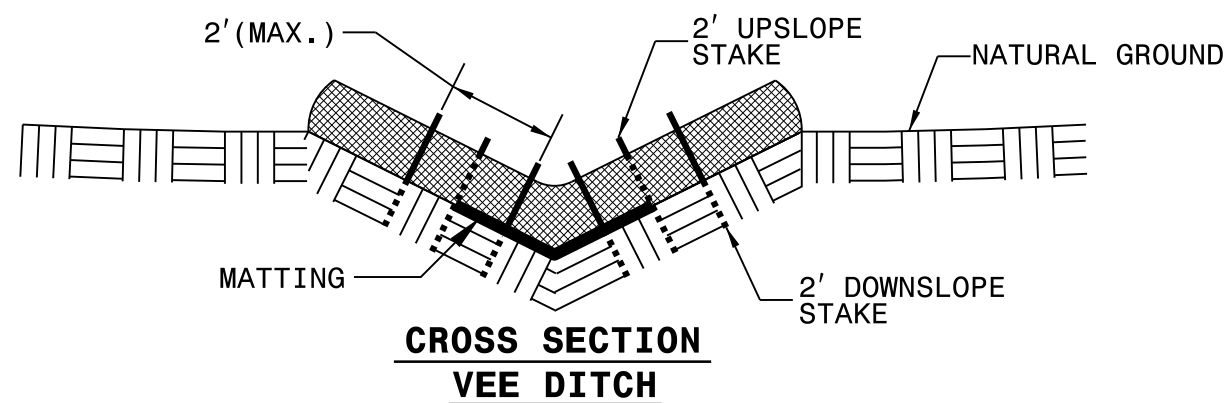
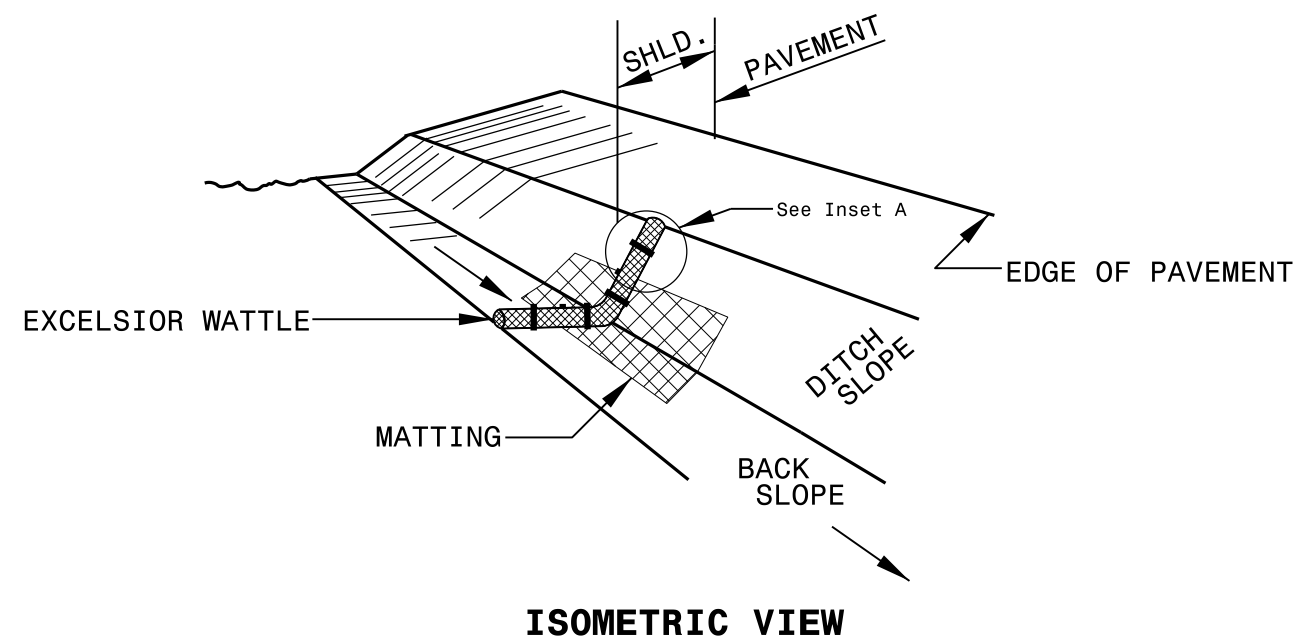


< 5' - 10' Undisturbed buffer from inlet, add wattle



NOT TO SCALE

# WATTLE WITH POLYACRYLAMIDE DETAIL



**NOTES:**

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. CROSS SECTION.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.  
INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.

PRIOR TO POLYACRYLAMIDE (PAM) APPLICATION, OBTAIN A SOIL SAMPLE FROM PROJECT LOCATION, AND FROM OFFSITE MATERIAL, AND ANALYZE FOR APPROPRIATE PAM FLOCCULANT TO BE APPLIED TO EACH WATTLE.

INITIALLY APPLY 3.5 OUNCES OF ANIONIC OR NEUTRALLY CHARGED POLYACRYLAMIDE (PAM) OVER WATTLE WHERE WATER WILL FLOW AND AFTER EVERY RAINFALL EVENT THAT IS EQUAL TO OR EXCEEDS 0.50 IN.

